

# A66 Northern Trans-Pennine project

Draft Construction  
Method and  
Management  
Statement

**Indicative  
schematic drawings  
Volume 2**

**Cross Lanes to Rokeby**



A66 - Key Proposed Construction Activities (Phase 1 Works)

**Legend**

Roads open & in public use

Construction Work Area Phase 1

Construction Work Area Phase 2

**Traffic Management for construction works**

Traffic will be diverted to enable works to take place with traffic management in use.

Construction Work Area

**Construction works - New carriageway**

With traffic diverted on the eastbound carriageway, works can take place on modifying and widening the westbound carriageway to suit the new road alignment. This will involve changes to the existing carriageway and widening works. Material excavated will be transported to local stockpile areas for reuse, with imported material brought in for the works. Kerbs and drainage will be installed, followed by aggregate and asphalt being compacted in layers to form the new road. The works will be undertaken away from live traffic with traffic management in place, to minimise disruption to the A66 traffic.

Construction Work Area

**Construction works - Overbridge**

The overbridge has been designed with sufficient working widths to enable the construction of the abutment whilst A66 traffic is maintained. Once complete it will help reduce vehicle movements turning, helping to improve traffic flows. The remaining part of the bridge will be constructed as part of the next phases of works, including the deck.

Construction Work Area

**Construction works - Embankment**

The embankment will be constructed from suitably selected fill materials, compacted in layers when complete the access road constructed. Drainage and kerbs will be followed by aggregate and asphalt being installed in layers to complete the new road surface.

Construction Work Area

**Construction works - New road and upgrade works**

Whilst the existing road remains operational, the new access roads will be constructed where possible, excluding the final connection elements. This will involve new kerbs, drainage and aggregate / asphalt material that is compacted in layers to form the new road. For some elements of work including the connection, additional traffic management is likely to be required.

Construction Work Area

**Important** - Some features shown on this drawing may be different from those shown in the Maps Books. The purpose of the drawings highlight elements of the construction works and where discrepancies with the Map Books exist, the Map Book versions should be regarded as showing the preliminary design.

**RESIDUAL DESIGN HAZARDS**

(The following information has been collected from Preconstruction Information and the Arup DJV CDM Hazard Management Process.)

**NOTES**

- All levels are in metres above Ordnance Datum.
- All dimensions are in metres unless otherwise stated.
- This drawing is to be read in conjunction with all other relevant drawings and the accompanying technical note HE565627-AMY-HGN-S08-TN-CH-000001.
- The drainage design is of a sufficient level of detail to advise land take requirements and early stage design feasibility. Further design development to be undertaken.
- The culvert naming convention is scheme specific for design coordination purposes only. A more detailed naming convention is to be agreed at a later design stage.
- The vehicle restraint system layout is a high level assessment and requires a detailed risk assessment (RRRAP) of the hazards present in the design.
- Road markings, lighting columns and structures are indicative only and require further design development.
- Traffic sign faces and locations are indicative only. Traffic signs will be developed at detailed design.
- Boundary treatments, such as fencing, are subject to agreement with landowners and detailed design.
- Proposed walking, cycling and horse riding routes are indicative only and subject to agreement with local authority and landowners.
- A high level assessment of technology requirements has been completed, with allowance made in the red line boundary for any new installations.

**KEY**

- Green line boundary
- Existing layout
- Existing watercourse
- Proposed layout
- Highway structure
- Culvert
- Cut-off drain
- Filter Drain
- Proposed boundary treatment
- Proposed gate
- Traffic sign
- Proposed lighting column
- Proposed vehicle restraint system
- Headwall
- Pipe to Outfall
- Earthworks
- Pond
- Proposed Bridleway
- Abandoned Bridleway
- Existing Bridleway
- Proposed Footway/Footpath
- Abandoned Footway/Footpath
- Existing Footway/Footpath
- Proposed Shared Cycle Footway
- Abandoned Shared Cycle Footway
- Existing Shared Cycle Footway
- Farm Track/Access
- Compound/Storage Area
- Demolished buildings
- AONB
- Potential Environmental Mitigation

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P01	ISSUED FOR MODEL FREEZE D	---	---	---	---
---	SHOT	SHOT	THAL	RPEA	---
---	19/02/21	19/02/21	19/02/21	19/02/21	---
P02.1	UPDATED FOR MODEL FREEZE E	---	---	---	---
---	---	---	---	---	---
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Revision	Revision details				
---	Created	Checked	Reviewed	Approved	Authorised
---	dd/mm/yy	dd/mm/yy	dd/mm/yy	dd/mm/yy	dd/mm/yy

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Integrated Project Team

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Manchester  
M1 3BN

**highways**  
england

Project Name  
A66 Northern Trans-Pennine

Drawing Title  
Scheme 8: Cross Lanes to Rokeby  
Black Option  
General Arrangement  
Sheet 1 of 5

Project Ref No  
---

Stage  
PCF3

Scale :  
1:1000

@ A0

Dimensions :  
M

Drawing Number  
Project | Originator | Volume |  
HE565627 - AMY - HGN -  
S08 - DR-CH - 600001  
Location | Type | Role | Number

Suitability  
S0

Suitability Description  
Initial non-contractual code

Revision  
P02.1



## A66 - Key Proposed Construction Activities (Phase 1 Works)

### Legend

## Roads open & in public use

## Construction Work Area Phase 1

## Construction Work Area Phase 2

## Construction works - Additional widening of A66

To enable the new A66 to be constructed safely away from live traffic, local construction of the east bound carriageway will be necessary. This will be undertaken, at this location, by moving the traffic to the Westbound carriageway and constructing the localised widening. Traffic will then be moved to the Eastbound carriageway so the westbound can be constructed.

**Important** Some features shown on this drawing may be different from those shown in the Maps Books. The purpose of the drawings highlight elements of the construction works and where discrepancies with the Map Books exist, the Map Book versions should be regarded as showing the preliminary design.
































## RESIDUAL DESIGN HAZARDS

(The following information has been collected from Preconstruction Information and the Arup Arup DJV CDM Hazard Management Process.)

## NOTES

1. All levels are in metres above Ordnance Datum.
2. All dimensions are in metres unless otherwise stated.
3. All dimensions are to be read from the centre of other relevant drawings and the accompanying technical note HES656721-AMY-HGN-S08-US-TCN-000001.
4. The design draught is of a sufficient level of detail to advise land take requirements and early stage project feasibility. Further development is to be undertaken.
5. The current naming convention is scheme specific for design development purposes only. Detailed naming convention is to be agreed at a later design stage.
6. The vehicle restraint system layout is a high level assessment and requires a detailed risk assessment (RRAP) of the hazards present in the design.
7. Road markings, lighting columns and structures are indicative only and requires further design development.
8. Traffic signs and road markings are indicative only. Traffic signs will be developed at detailed design.
9. Boundary treatments, such as fencing, are subject to agreement with the local authority and landowners.
10. Proposed walking, cycling and horse riding routes are indicative only and subject to agreement with local authority and landowners.
11. A high level assessment of technology requirements has been completed, with allowance made in the red line boundary for any new installations.

## KEY

- |   |                                    |
|---|------------------------------------|
|  | Green line boundary                |
|  | Existing layout                    |
|  | Existing watercourse               |
|  | Proposed layout                    |
|  | Highway structure                  |
|  | Culvert                            |
|  | Cut-off drain                      |
|  | Filter Drain                       |
|  | Proposed boundary treatment        |
|  | Proposed gate                      |
|  | Traffic sign                       |
|  | Proposed lighting column           |
|  | Proposed vehicle restraint system  |
|  | Headwall                           |
|  | Pipe to Outfall                    |
|  | Earthworks                         |
|  | Pond                               |
|  | Proposed Bridleway                 |
|  | Abandoned Bridleway                |
|  | Existing Bridleway                 |
|  | Proposed Footway/Footpath          |
|  | Abandoned Footway/Footpath         |
|  | Existing Footway/Footpath          |
|  | Proposed Shared Cycle Footway      |
|  | Abandoned Shared Cycle Footway     |
|  | Existing Shared Cycle Footway      |
|  | Farm Track/Access                  |
|  | Compound/Storage Area              |
|  | Demolished buildings               |
|  | AO&B                               |
|  | Potential Environmental Mitigation |

## Construction works - New carriageway

With the localised widening completed and traffic diverted to the eastbound carriageway, works can take place on modifying and widening the westbound carriageway to suit the new road alignment. This will involve changes to the existing carriageway and widening works. Material excavated will be transported to local stockpile areas for reuse, with imported material brought in for the works. Kerbs and drainage will be installed, followed by aggregate and asphalt being compacted in layers to form the new road. The works will look to be undertaken away from live traffic with traffic management in place, to minimise disruption to the A66 traffic.

### Construction works - Attenuation pond

To help manage groundwater on the scheme and mitigate the impacts of surface run-off overloading the existing drainage network, attenuation ponds are required. They will be construed with minimal impacts on the live A66 and will further help biodiversity within the local area. Plant machinery will be used to excavate and line the attenuation pond.

[illegible]

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Project Name  
A66 Northern Trans-Pennine

Drawing Title  
Scheme 8: Cross Lanes to Rokeby  
Black Option  
General Arrangement  
Sheet 2 of 5

Project Ref. No.	Stage	Scale : 1:1000 @ A0
---	PCF3	Dimensions : M

Drawing Number			
Project	Originator	Volume	
HE565627	- AMY	- HGN	-
S08	-DR-CH- 600002		
Location	Type	Role	Number

Suitability	Suitability Description	Revision
S0	Initial non-contractual code	P02.1



# A66 - Key Proposed Construction Activities (Phase 1 Works)

**Legend**

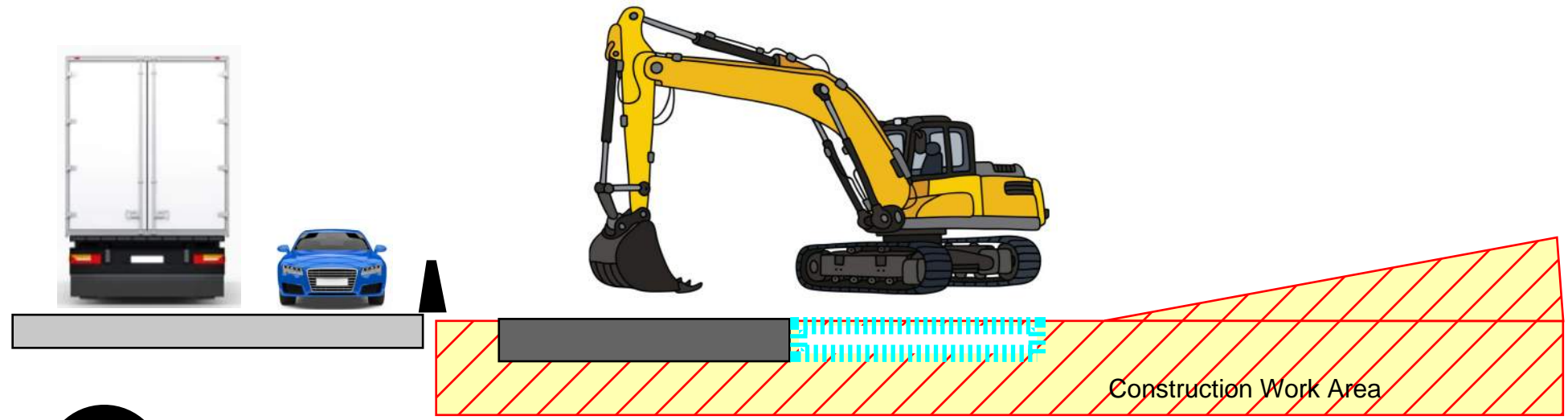
Roads open & in public use

Construction Work Area Phase 1

Construction Work Area Phase 2

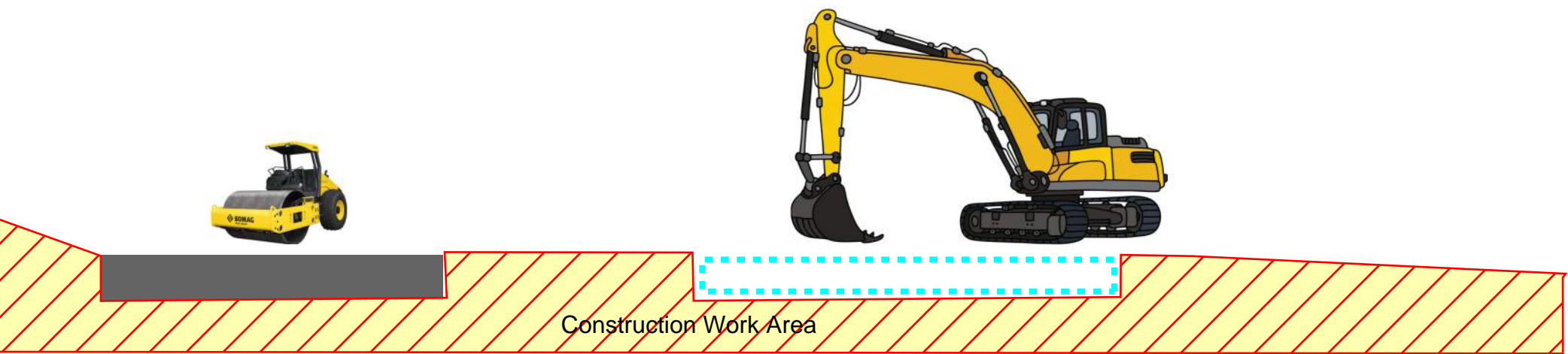
## Construction works - New carriageway

With traffic diverted on the eastbound carriageway, works can take place on modifying and widening the westbound carriageway to suit the new road alignment. This will involve changes to the existing carriageway and widening works. Material excavated will be transported to local stockpile areas for reuse, with imported material brought in for the works. Kerbs and drainage will be installed, followed by aggregate and asphalt being compacted in layers to form the new road. The works will look to be undertaken away from live traffic with traffic management in place, to minimise disruption to the A66 traffic.



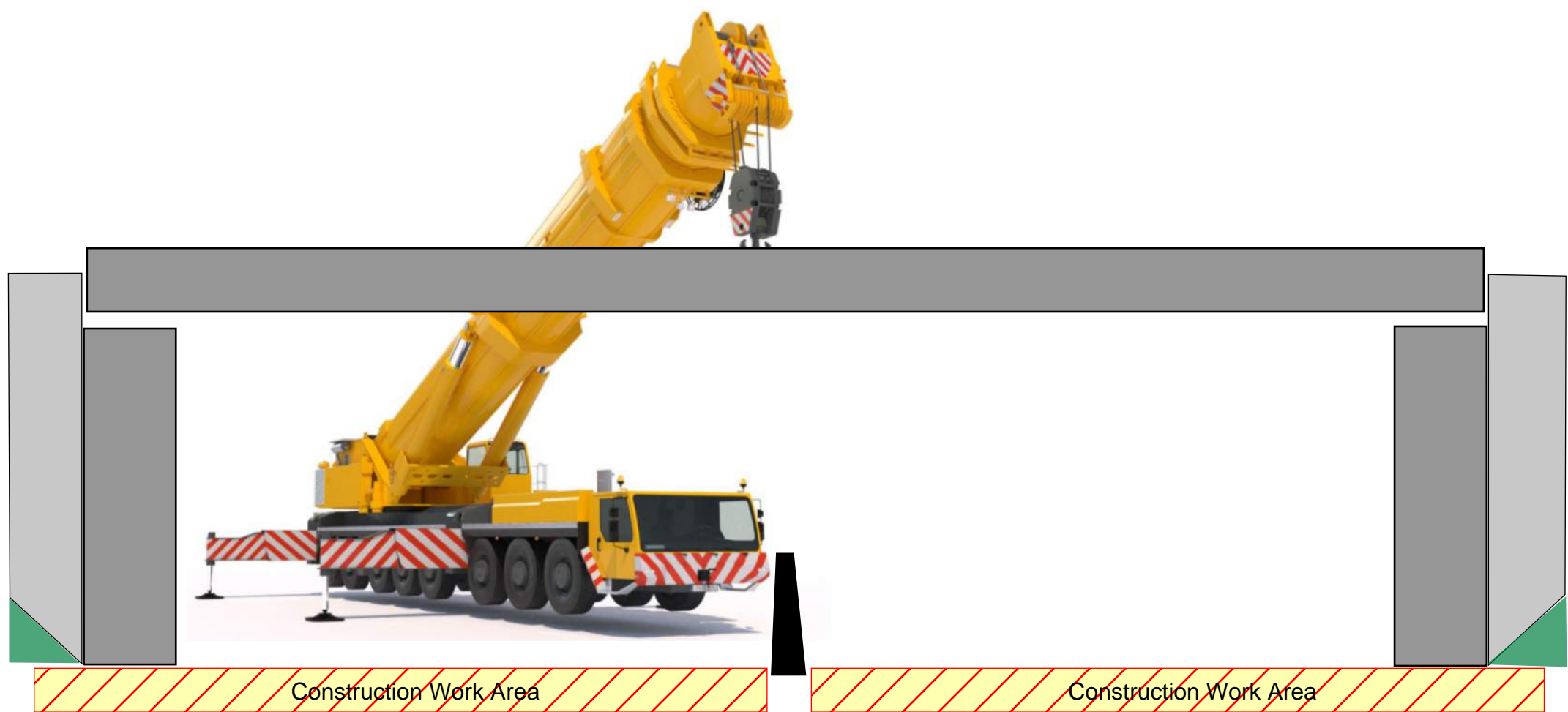
## Construction works - New carriageway construction

With traffic maintained on the existing A66 route, construction works can take place on the new carriageway. Material will be excavated from the works location and moved to stockpile locations on the project. Material will be moved via eight wheel wagons or dump trucks with suitable routes established to minimise disruption to the A66. The new road will be constructed from imported stone and asphalt material, with kerbs and drainage also being installed to complete the new road.



## Construction works - Overbridge Construction

As the new overbridge is well away from the existing A66 both abutments and the deck can be constructed in one phase of works, minimising disruption to local traffic. With the existing roads location, it is likely that the bridge beams can be installed with limited additional traffic management.



**Important** - Some features shown on this drawing may be different from those shown in the Maps Books. The purpose of the drawings highlight elements of the construction works and where discrepancies with the Map Books exist, the Map Book versions should be regarded as showing the preliminary design.

**RESIDUAL DESIGN HAZARDS**

(The following information has been collected from Preconstruction Information and the Amey Arup DJV CDM Hazard Management Process.)

### NOTES

- All levels are in metres above Ordnance Datum.
- All dimensions are in metres unless otherwise stated.
- This drawing is to be read in conjunction with all other relevant drawings and the accompanying technical note HE565627-AMY-HGN-S08-TN-CH-000001.
- The drainage design is of a sufficient level of detail to advise land take requirements and early stage design feasibility. Further design development to be undertaken.
- The culvert naming convention is scheme specific for design coordination purposes only. A more detailed naming convention is to be agreed at a later design stage.
- The vehicle restraint system layout is a high level assessment and requires a detailed risk assessment (RRRAP) of the hazards present in the design.
- Road markings, lighting columns and structures are indicative only and require further design development.
- Traffic sign faces and locations are indicative only. Traffic signs will be developed at detailed design.
- Boundary treatments, such as fencing, are subject to agreement with landowners and detailed design.
- Proposed walking, cycling and horse riding routes are indicative only and subject to agreement with local authority and landowners.
- A high level assessment of technology requirements has been completed, with allowance made in the red line boundary for any new installations.

### KEY

- Green line boundary
- Existing layout
- Existing watercourse
- Proposed layout
- Highway structure
- Culvert
- Cut-off drain
- Filter Drain
- Proposed boundary treatment
- Proposed gate
- Traffic sign
- Proposed lighting column
- Proposed vehicle restraint system
- Headwall
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- Earthworks
- Pond
- Proposed Bridleway
- Abandoned Bridleway
- Existing Bridleway
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- Abandoned Shared Cycle Footway
- Existing Shared Cycle Footway
- Farm Track/Access
- Compound/Storage Area
- Demolished buildings
- AONB
- Potential Environmental Mitigation

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ISSUED FOR MODEL FREEZE D					
P01	SHOT	SHOT	THAL	RPEA	---
	19/02/21	19/02/21	19/02/21	19/02/21	---
UPDATED FOR MODEL FREEZE E					
P02.1	---	---	---	---	---
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Revision details					
Revision	Created	Checked	Reviewed	Approved	Authorised
	dd/mm/yy	dd/mm/yy	dd/mm/yy	dd/mm/yy	dd/mm/yy

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Project Name

A66 Northern Trans-Pennine

Drawing Title

Scheme 8: Cross Lanes to Rokeby  
Black Option  
General Arrangement  
Sheet 3 of 5

Project Ref. No.

Stage

Scale : 1:1000 @ A0

Dimensions : M

Drawing Number

Project

Originator

Volume

HE565627 - AMY - HGN - S08 -DR-CH - 6000003

Location

Type

Role

Number

Suitability

Suitability Description

Revision

S0 Initial non-contractual code P02.1



## **A66 - Key Proposed Construction Activities (Phase 1 Works)**

### Legend

## Roads open & in public use

## Construction Work Area Phase 1

## Construction Work Area Phase 2

## Construction works - New access route

The new road will be excavated to a suitable formation level and a new granular and asphalt layers installed to form the road, following the drainage and kerb works. Excavated material will be sent to stockpiles and reused where suitable.

**Important** Some features shown on this drawing may be different from those shown in the Maps Books. The purpose of the drawings highlight elements of the construction works and where discrepancies with the Map Books exist, the Map Book versions should be regarded as showing the preliminary design.
































## RESIDUAL DESIGN HAZARDS

(The following information has been collected from Preconstruction Information and the Arup Arup DJV CDM Hazard Management Process.)

## NOTES

1. All levels are in metres above Ordinance Datum.
2. All drawings are in plan.
3. This drawing is to be read in conjunction with all other relevant drawings and the accompanying technical note E5069072-ANY AND SITE.
4. The drainage design is of a sufficient level of detail to advise land take requirements and early stage design feasibility. Further design details are to be undertaken by the client.
5. The current culvert numbering is scheme specific for design coordination purposes only. A more detailed naming convention is to be agreed at a later design stage.
6. The vehicle restraint system layout is a high level assessment and does not include a detailed risk assessment (RRAP) of the hazards present in the design.
7. Road markings, lighting columns and structures are indicative only and are to be confirmed by the client.
8. Traffic sign faces and locations are indicative only. Traffic signs will be developed at detailed design.
9. All fencing, including any security fencing, are subject to agreement with landowners and detailed design.
10. All existing, cycling, walking and horse riding routes are indicative only and subject to agreement with local authority and landowners.
11. A high level assessment of technology requirements has been completed, with allowance made in the red line boundary for any new installations.

## KEY

- |   |                                    |
|---|------------------------------------|
|  | Green line boundary                |
|  | Existing layout                    |
|  | Existing watercourse               |
|  | Proposed layout                    |
|  | Highway structure                  |
|  | Culvert                            |
|  | Cut-off drain                      |
|  | Filter Drain                       |
|  | Proposed boundary treatment        |
|  | Proposed gate                      |
|  | Traffic sign                       |
|  | Proposed lighting column           |
|  | Proposed vehicle restraint system  |
|  | Headwall                           |
|  | Pipe to Outfall                    |
|  | Earthworks                         |
|  | Pond                               |
|  | Proposed Bridleway                 |
|  | Abandoned Bridleway                |
|  | Existing Bridleway                 |
|  | Proposed Footway/Footpath          |
|  | Abandoned Footway/Footpath         |
|  | Existing Footway/Footpath          |
|  | Proposed Shared Cycle Footway      |
|  | Abandoned Shared Cycle Footway     |
|  | Existing Shared Cycle Footway      |
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|  | Compound/Storage Area              |
|  | Demolished buildings               |
|  | AONB                               |
|  | Potential Environmental Mitigation |

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P01	ISSUED FOR MODEL FREEZE D					
	SHOT	SHOT	THAL	RPEA		
	19/02/21	19/02/21	19/02/21	19/02/21		
P02.1	UPDATED FOR MODEL FREEZE E					
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Project Name  
A66 Northern Trans-Pennine

Drawing Title  
 Scheme 8: Cross Lanes to Rokeyby  
 Black Option  
 General Arrangement  
 Sheet 4 of 5

Project Ref. No.	Stage	Scale : 1:1000 @ A0
---	PCF3	Dimensions : M

Drawing Number			
Project	Originator	Volume	
HE565627 - AMY - HGN -			
S08	-DR-CH- 600004		
Location	Type	Role	Number

Suitability	Suitability Description	Revision
S0	Initial non-contractual code	P02.1

## Construction works - Attenuation ponds

To help manage groundwater on the scheme and mitigate the impacts of surface run-off overloading the existing drainage network, attenuation ponds are required. They will be constructed with minimal impacts on the live A66 and will further help biodiversity within the local area. Plant machinery will be used to excavate and line the attenuation pond.

### Construction works - New carriageway construction

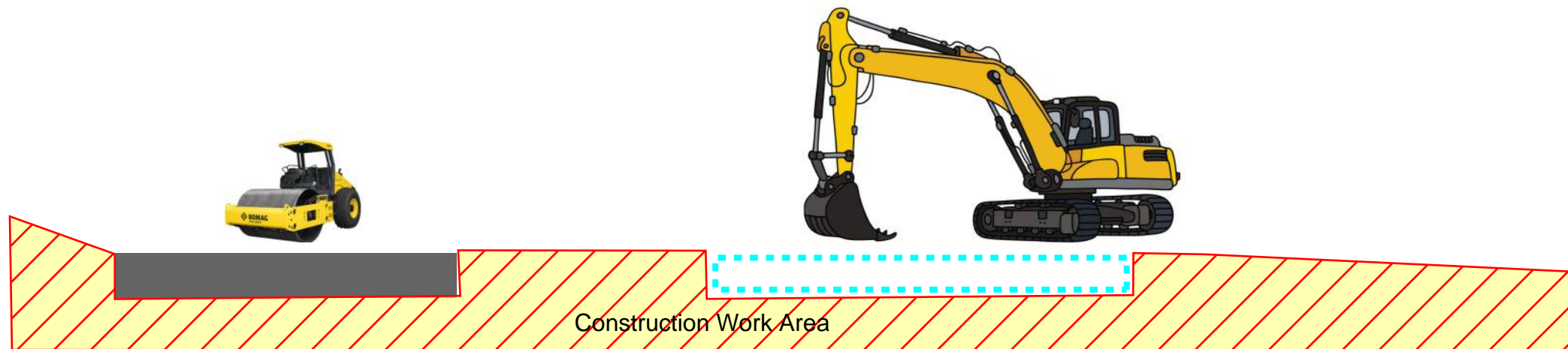
With traffic maintained on the existing A66 route, construction works can take place on the new carriageway. Material will be excavated from the works location and moved to stockpile locations on the project. Material will be moved via eight wheel wagons or dump trucks with suitable routes established to minimise disruption to the A66. The new road will be constructed from imported stone and asphalt material, with kerbs and drainage also being installed to complete the new road.



# A66 - Key Proposed Construction Activities (Phase 1 Works)

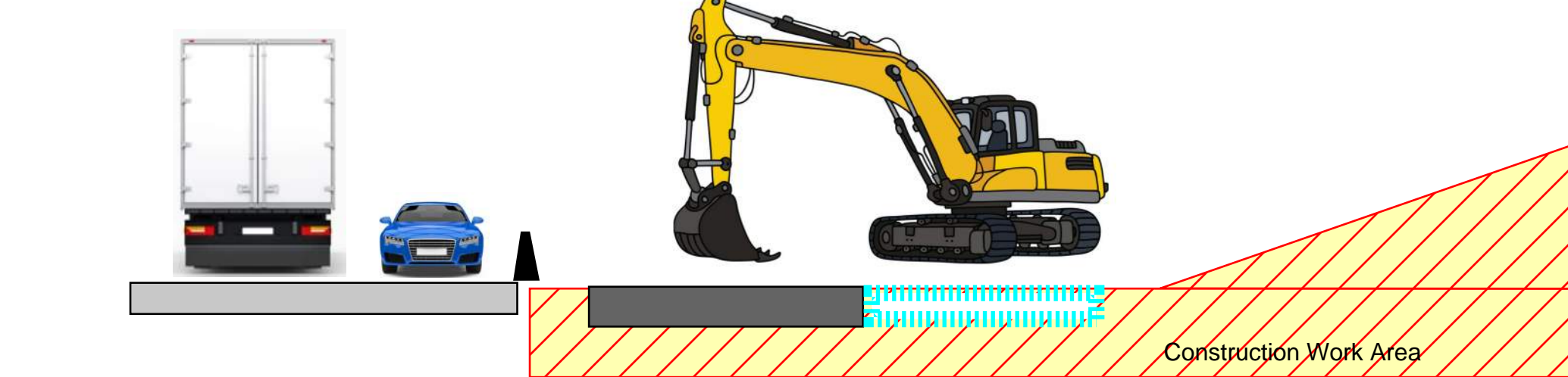
## Construction works - New carriageway construction

With traffic maintained on the existing A66 route, construction works can take place on the new carriageway. Material will be excavated from the works location and moved to stockpile locations on the project. Material will be moved via eight wheel wagons or dump trucks with suitable routes established to minimise disruption to the A66. The new road will be constructed from imported stone and asphalt material, with kerbs and drainage also being installed to complete the new road.



## Construction works - New carriageway

With traffic diverted on the eastbound carriageway, works can take place on modifying and widening the westbound carriageway to suit the new road alignment. This will involve changes to the existing carriageway and widening works. Material excavated will be transported to local stockpile areas for reuse, with imported material brought in for the works. Kerbs and drainage will be installed, followed by aggregate and asphalt being compacted in layers to form the new road. The works will look to be undertaken away from live traffic with traffic management in place, to minimise disruption to the A66 traffic.



For continuation refer to drawing 600005  
For continuation refer to drawing 600004

A66 to be de-trunked

Existing access to Tack Room Cottage stopped-up

Trough

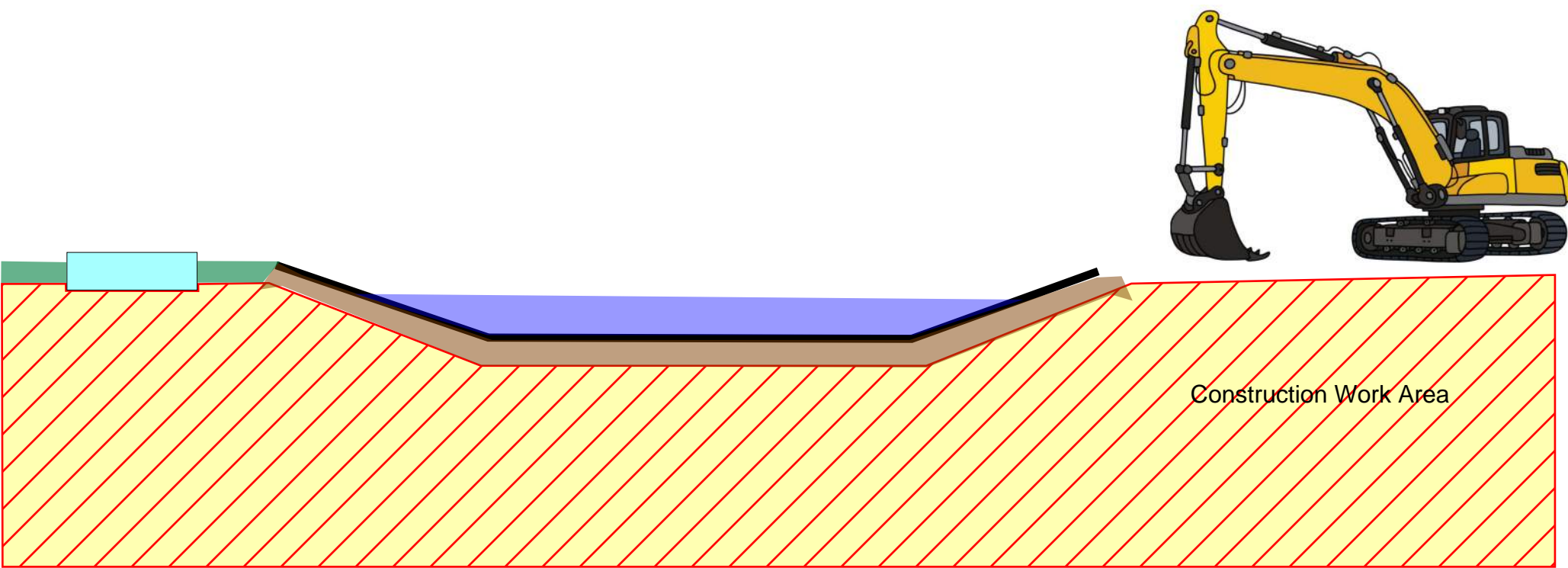
Lay-by

Tack Room Cottage  
The Grove

Track

## Construction works - Attenuation pond

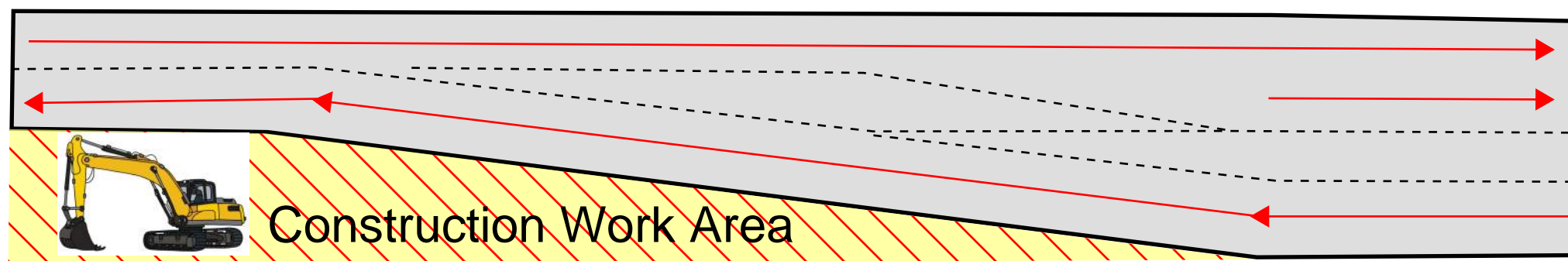
To help manage groundwater on the scheme and mitigate the impacts of surface run-off overloading the existing drainage network, attenuation ponds are required. They will be construed with minimal impacts on the live A66 and will further help biodiversity within the local area. Plant machinery will be used to excavate and line the attenuation pond.



**Important** - Some features shown on this drawing may be different from those shown in the Maps Books. The purpose of the drawings highlight elements of the construction works and where discrepancies with the Map Books exist, the Map Book versions should be regarded as showing the preliminary design.

## Traffic management for construction works

Traffic will managed by the traffic management to enable works to take place safely.



### Legend

Roads open & in public use

Construction Work Area Phase 1

Construction Work Area Phase 2

### RESIDUAL DESIGN HAZARDS

(The following information has been collected from Preconstruction Information and the Amey Arup DJV CDM Hazard Management Process.)

### NOTES

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- All dimensions are in metres unless otherwise stated.
- This drawing is to be read in conjunction with all other relevant drawings and the accompanying technical note HE565627-AMY-HGN-S08-TN-CH-000001.
- The drainage design is of a sufficient level of detail to advise land take requirements and early stage design feasibility. Further design development to be undertaken.
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- The vehicle restraint system layout is a high level assessment and requires a detailed risk assessment (RRRAP) of the hazards present in the design.
- Road markings, lighting columns and structures are indicative only and require further design development.
- Traffic sign faces and locations are indicative only. Traffic signs will be developed at detailed design.
- Boundary treatments, such as fencing, are subject to agreement with landowners and detailed design.
- Proposed walking, cycling and horse riding routes are indicative only and subject to agreement with local authority and landowners.
- A high level assessment of technology requirements has been completed, with allowance made in the red line boundary for any new installations.

### KEY

- Green line boundary
- Existing layout
- Existing watercourse
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- Cut-off drain
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- Potential Environmental Mitigation

Revision	Created	Checked	Reviewed	Approved	Authorised
---	dd/mm/yy	dd/mm/yy	dd/mm/yy	dd/mm/yy	dd/mm/yy

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**highways**  
england

Project Name  
A66 Northern Trans-Pennine

Drawing Title  
Scheme 8: Cross Lanes to Rokeby  
Black Option  
General Arrangement  
Sheet 5 of 5

Project Ref. No. Stage Scale : 1:1000 @ A0  
--- PCF3 Dimensions : M

Drawing Number  
Project HE565627 - AMY - HGN - S08  
Location - DR-CH - 600005  
I Type I Role I Number

Suitability S0 Initial non-contractual code Revision P02.1

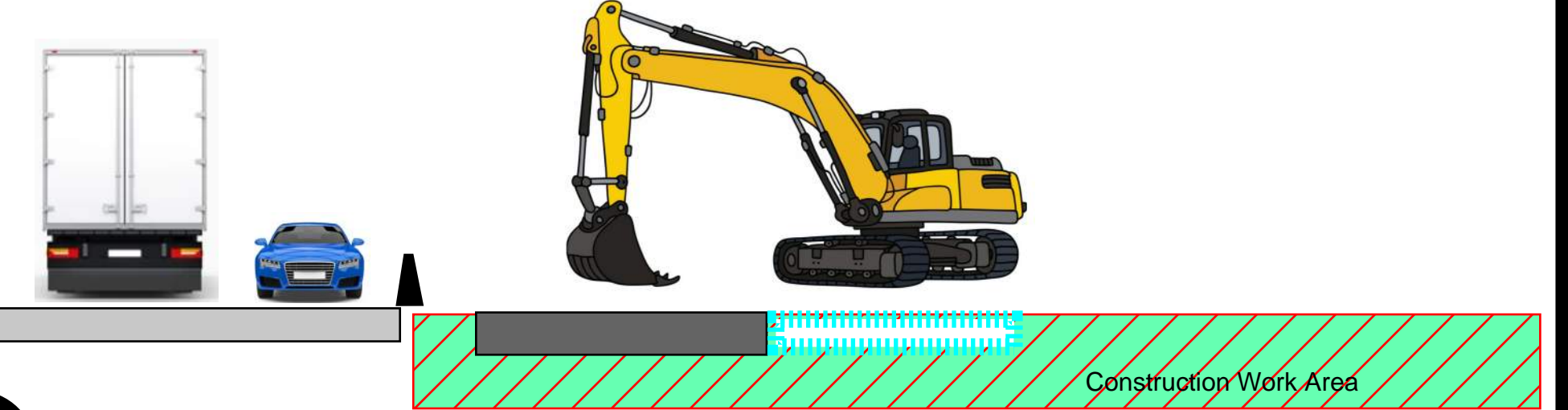


# A66 - Key Proposed Construction Activities (Phase 2 Works)

**Important** - Some features shown on this drawing may be different from those shown in the Maps Books. The purpose of the drawings highlight elements of the construction works and where discrepancies with the Map Books exist, the Map Book versions should be regarded as showing the preliminary design.

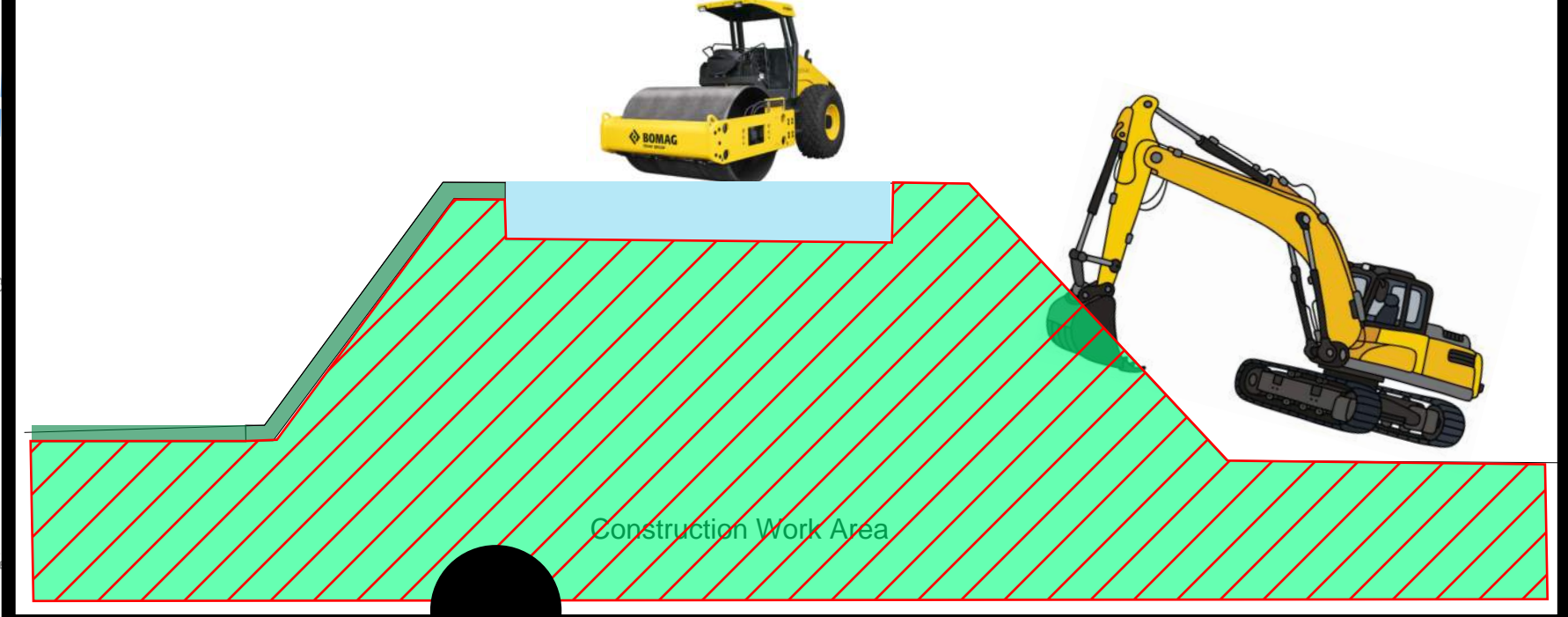
### Construction works - New carriageway

With traffic diverted on the westbound carriageway, works can take place on modifying and widening the eastbound carriageway to suit the new road alignment. This will involve changes to the existing carriageway and widening works. Excavated material will be transported to local stockpile areas for reuse, with imported aggregate and asphalt material brought in for the works. Kerbs and drainage will be installed, followed by the new road being constructed.



### Construction works - Embankment

The access road will help mitigate movements across the A66 once complete. The embankment will be constructed in layers from suitably selected fill materials. Kerbs and drainage will then be installed, followed by the new road consisting of aggregate and asphalt.

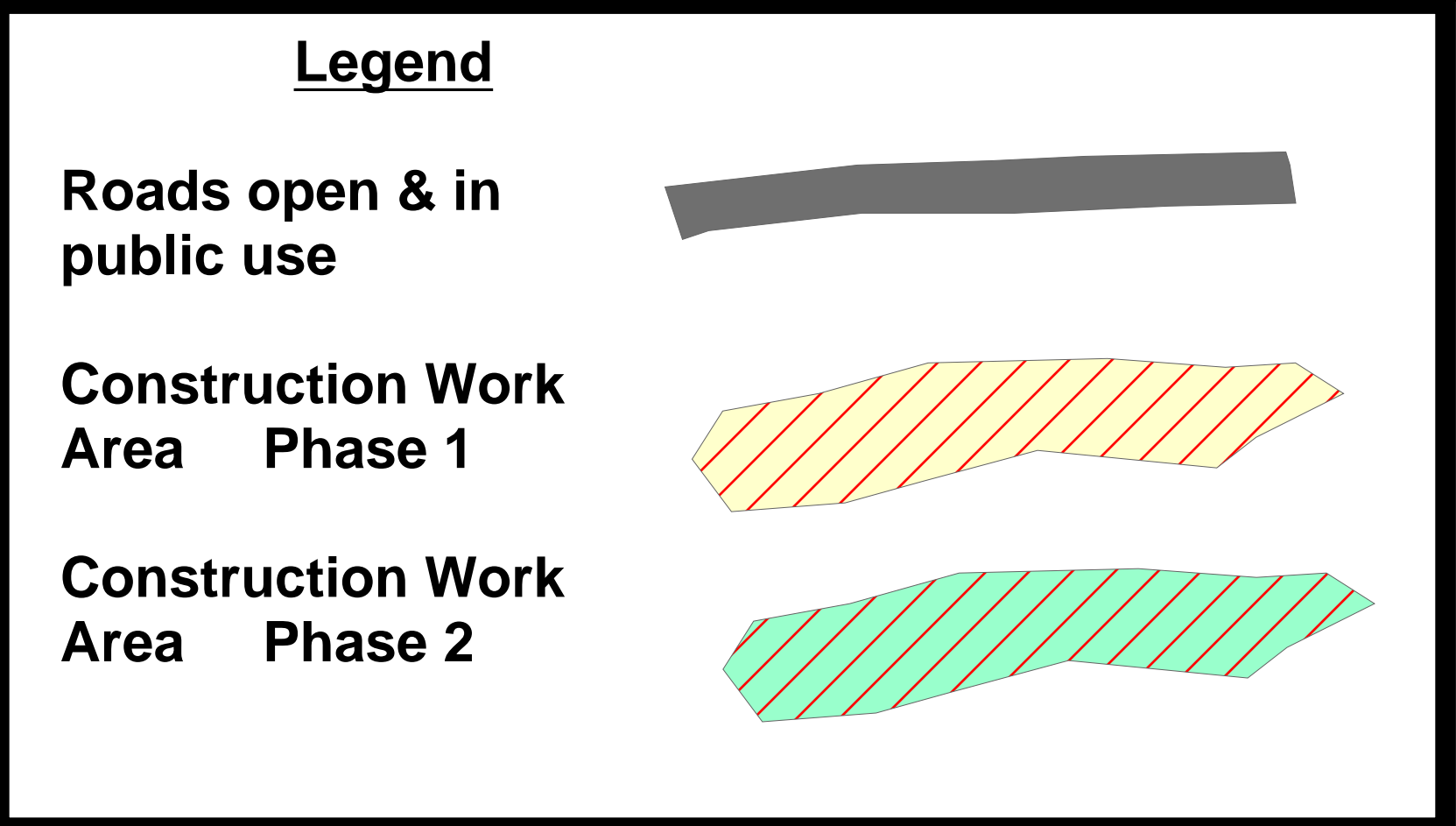


### Legend

Roads open & in public use

Construction Work Area Phase 1

Construction Work Area Phase 2



### RESIDUAL DESIGN HAZARDS

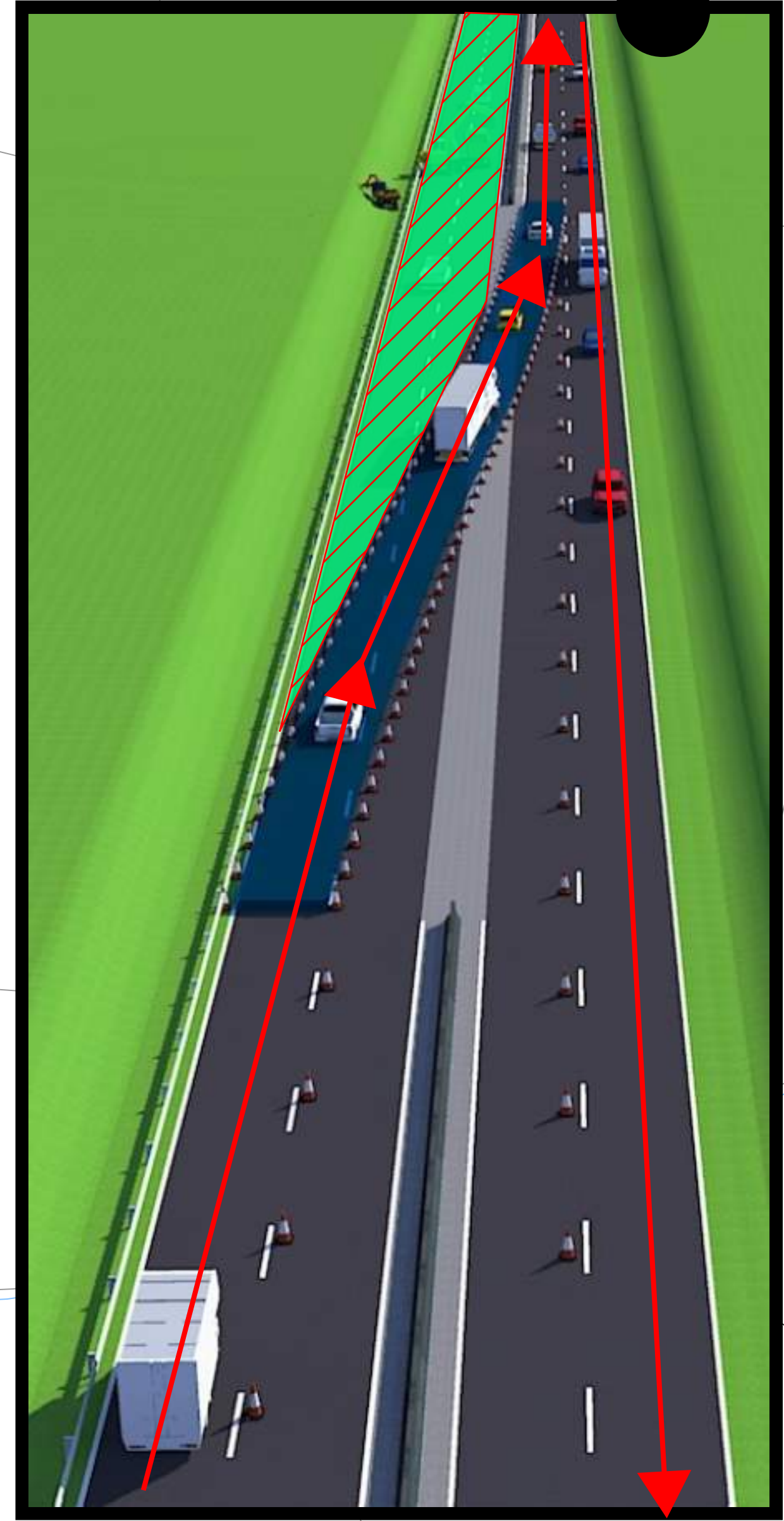
(The following information has been collected from Preconstruction Information and the Arup DJV CDM Hazard Management Process.)

### NOTES

- All levels are in metres above Ordnance Datum.
- All dimensions are in metres unless otherwise stated.
- This drawing is to be read in conjunction with all other relevant drawings and the accompanying technical note HE565627-AMY-HGN-S08-TN-CH-000001.
- The drainage design is of a sufficient level of detail to advise land take requirements and early stage design feasibility. Further design development to be undertaken.
- The culvert naming convention is scheme specific for design coordination purposes only. A more detailed naming convention is to be agreed at a later design stage.
- The vehicle restraint system layout is a high level assessment and requires a detailed risk assessment (RRRAP) of the hazards present in the design.
- Road markings, lighting columns and structures are indicative only and require further design development.
- Traffic sign faces and locations are indicative only. Traffic signs will be developed at detailed design.
- Boundary treatments, such as fencing, are subject to agreement with landowners and detailed design.
- Proposed walking, cycling and horse riding routes are indicative only and subject to agreement with local authority and landowners.
- A high level assessment of technology requirements has been completed, with allowance made in the red line boundary for any new installations.

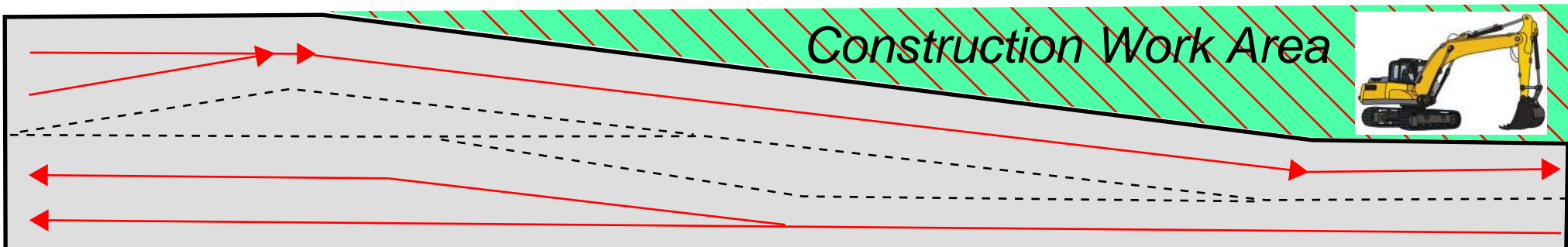
### KEY

- Green line boundary
- Existing layout
- Existing watercourse
- Proposed layout
- Highway structure
- Culvert
- Cut-off drain
- Filter Drain
- Proposed boundary treatment
- Proposed gate
- Traffic sign
- Proposed lighting column
- Proposed vehicle restraint system
- Headwall
- Pipe to Outfall
- Earthworks
- Pond
- Proposed Bridleway
- Abandoned Bridleway
- Existing Bridleway
- Proposed Footway/Footpath
- Abandoned Footway/Footpath
- Existing Footway/Footpath
- Proposed Shared Cycle Footway
- Abandoned Shared Cycle Footway
- Existing Shared Cycle Footway
- Farm Track/Access
- Compound/Storage Area
- Demolished buildings
- AONB
- Potential Environmental Mitigation



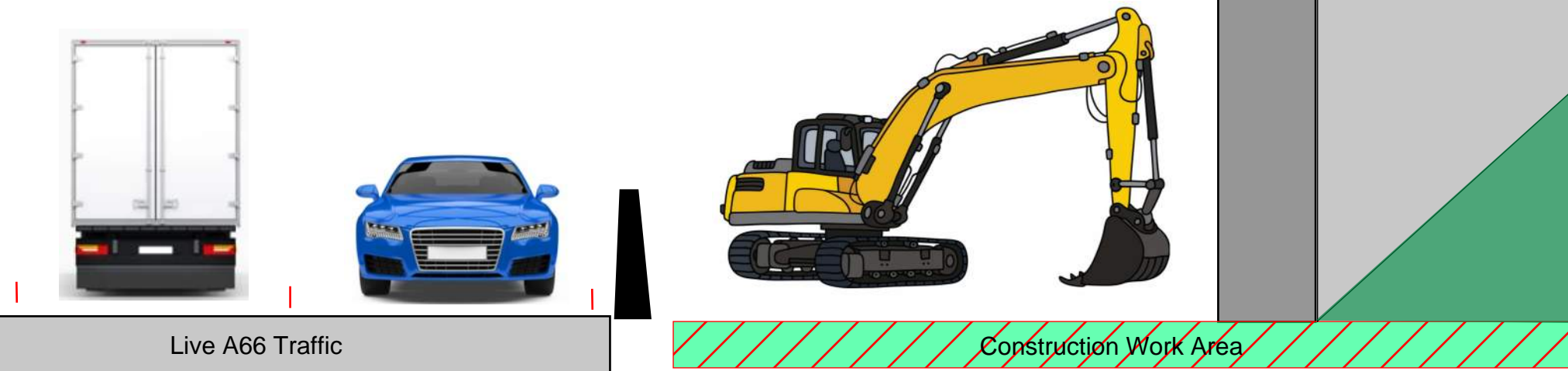
### Traffic management for construction works

Traffic will be relocated onto part of the new A66 carriageway to enable the remaining works to safely take place.

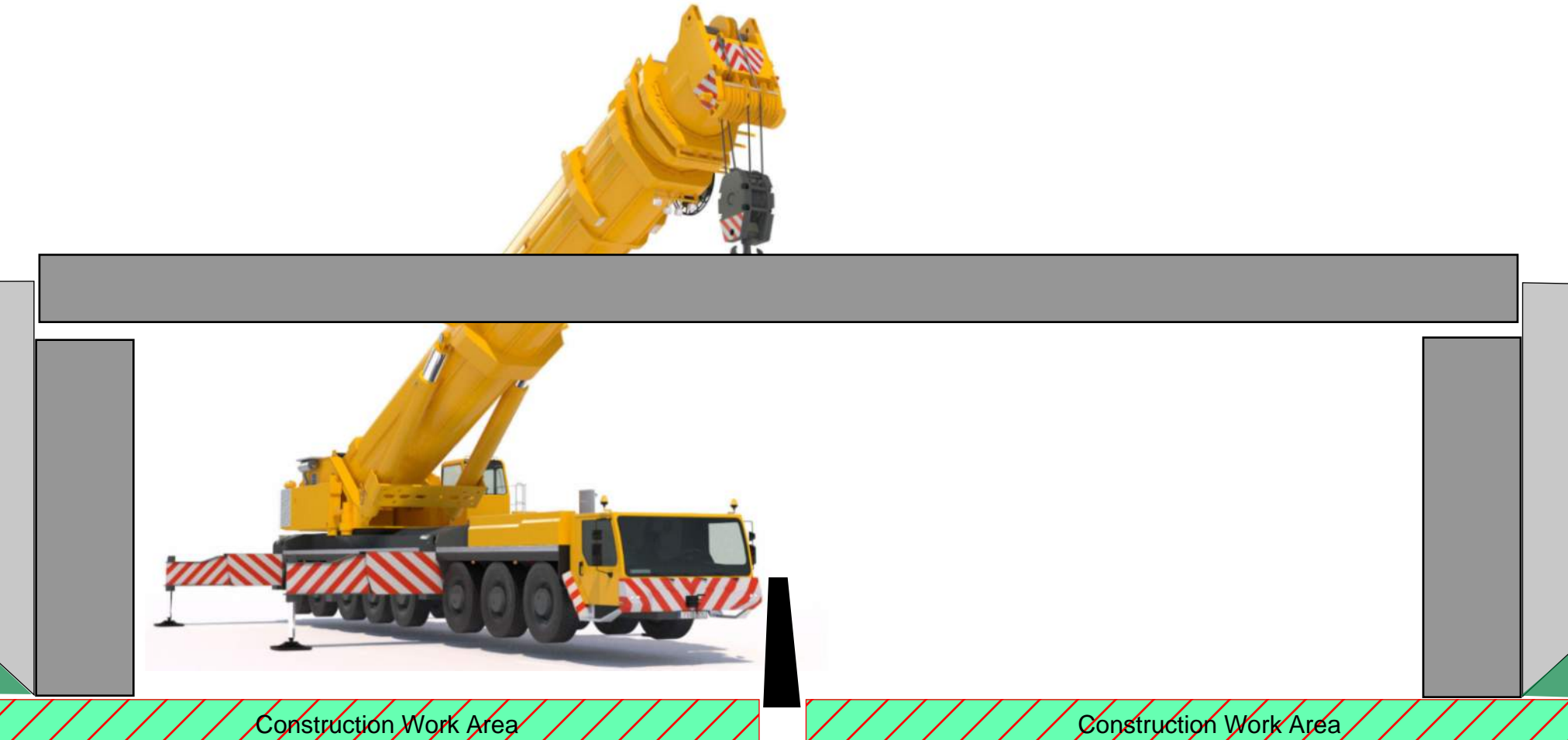


### Construction works - Overbridge

The overbridge has been designed with sufficient working widths to enable the construction of the abutment whilst A66 traffic is maintained.



Traffic management will be required to complete the deck with disruption being minimised. Once in place the road can be installed.



Revision	Created	Checked	Reviewed	Approved	Authorised
---	---	---	---	---	---

Designer  
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Client  
3 Piccadilly Place  
Manchester  
M1 3BN

Project Name  
A66 Northern Trans-Pennine

Drawing Title  
Scheme 8: Cross Lanes to Rokeby  
Black Option  
General Arrangement  
Sheet 1 of 5

Project Ref. No. Stage Scale: 1:1000 @ A0  
--- PCF3 Dimensions: M

Drawing Number  
Project | Originator | Volume |  
HE565627 - AMY - HGN -  
S08 -DR-CH- 600001  
Location | Type | Role | Number

Suitability Suitability Description Revision  
S0 Initial non-contractual code P02.1



## A66 - Key Proposed Construction Activities (Phase 2 Works)

### Legend

## Roads open & in public use

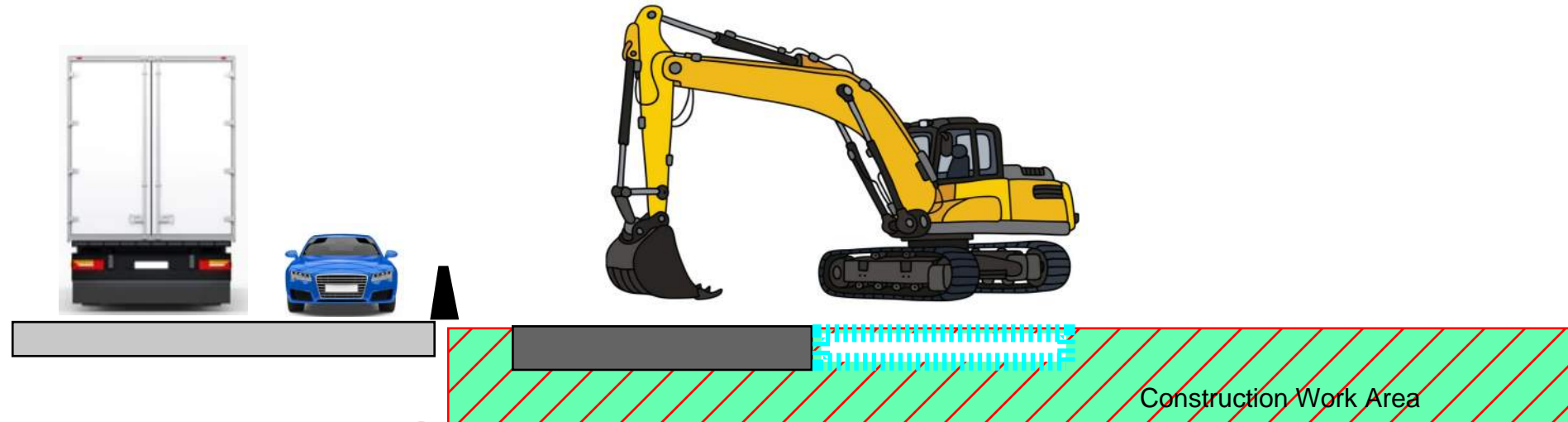
## Construction Work Area Phase 1

## Construction Work Area Phase 2

**Important** Some features shown on this drawing may be different from those shown in the Maps Books. The purpose of the drawings highlight elements of the construction works and where discrepancies with the Map Books exist, the Map Book versions should be regarded as showing the preliminary design.

## Construction works - New carriageway

With traffic diverted on the westbound carriageway, works can take place on modifying and widening the eastbound carriageway to suit the new road alignment. This will involve changes to the existing carriageway and widening works. Excavated material will be transported to local stockpile areas for reuse, with imported aggregate and asphalt material brought in for the works. Kerbs and drainage will be installed, followed by the new road being constructed with the aggregate and asphalt.


































## RESIDUAL DESIGN HAZARDS

(The following information has been collected from Preconstruction Information and the Amey Arup DJV CDM Hazard Management Process.)

## NOTES

1. All levels are in metres above Ordinance Datum.
2. All drawings are in plan unless otherwise stated.
3. This drawing is to be read in conjunction with all other relevant drawings and the accompanying technical note.
4. **MS60902, AWWA M22.2-01, BS 5958-1:2002**
5. The drainage design is of a sufficient level of detail to advise land take requirements and early stage design feasibility. Further design details are to be undertaken by the design team.
6. The current naming convention is scheme specific for design coordination purposes only. A more detailed naming convention is to be agreed at a later design stage.
7. The vehicle restraint system layout is a high level assessment and does not include detailed risk assessment (RRAP) of the hazards present in the design.
8. Road markings, lighting columns and structures are indicative only and subject to design development.
9. Traffic sign faces and locations are indicative only. Traffic signs will be developed at detailed design.
10. All fencing, including any security fencing, are subject to agreement with landowners and detailed design.
11. Pedestrian, cycling and horse riding routes are indicative only and subject to agreement with local authority and landowners.
12. A high level assessment of technology requirements has been completed with allowance made in the red line boundary for any new installations.

## KEY

- |   |                                    |
|---|------------------------------------|
|  | Green line boundary                |
|  | Existing layout                    |
|  | Existing watercourse               |
|  | Proposed layout                    |
|  | Highway structure                  |
|  | Culvert                            |
|  | Cut-off drain                      |
|  | Filter Drain                       |
|  | Proposed boundary treatment        |
|  | Proposed gate                      |
|  | Traffic sign                       |
|  | Proposed lighting column           |
|  | Proposed vehicle restraint system  |
|  | Headwall                           |
|  | Pipe to Outfall                    |
|  | Earthworks                         |
|  | Pond                               |
|  | Proposed Bridleway                 |
|  | Abandoned Bridleway                |
|  | Existing Bridleway                 |
|  | Proposed Footway/Footpath          |
|  | Abandoned Footway/Footpath         |
|  | Existing Footway/Footpath          |
|  | Proposed Shared Cycle Footway      |
|  | Abandoned Shared Cycle Footway     |
|  | Existing Shared Cycle Footway      |
|  | Farm Track/Access                  |
|  | Compound/Storage Area              |
|  | Demolished buildings               |
|  | AONB                               |
|  | Potential Environmental Mitigation |

P01	ISSUED FOR MODEL FREEZE D				
	SHOT	SHOT	THAL	RPEA	---
	19/02/21	19/02/21	19/02/21	19/02/21	---
	UPDATED FOR MODEL FREEZE E				
P02.1	---	---	---	---	---
Revision	Revision details				
	Created	Checked	Reviewed	Approved	Authorised
	dd/mm/vv	dd/mm/vv	dd/mm/vv	dd/mm/vv	dd/mm/vv

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**A66 NTP** **Integrated Project Team**

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Manchester  
M1 3BN



Project Name  
A66 Northern Trans-Pennine

Drawing Title  
Scheme 8: Cross Lanes to Rokeyby  
Black Option  
General Arrangement  
Sheet 2 of 5

Project Ref. No.	Stage	Scale : 1:1000 @ A0
---	PCF3	Dimensions : M

Drawing Number			
Project	Originator	Volume	
HE565627	- AMY	- HGN	-
S08	-DR-CH- 600002		
Location	Type	Role	Number

Suitability	Suitability Description	Revision
S0	Initial non-contractual code	P02.1



# A66 - Key Proposed Construction Activities (Phase 2 Works)

**Legend**

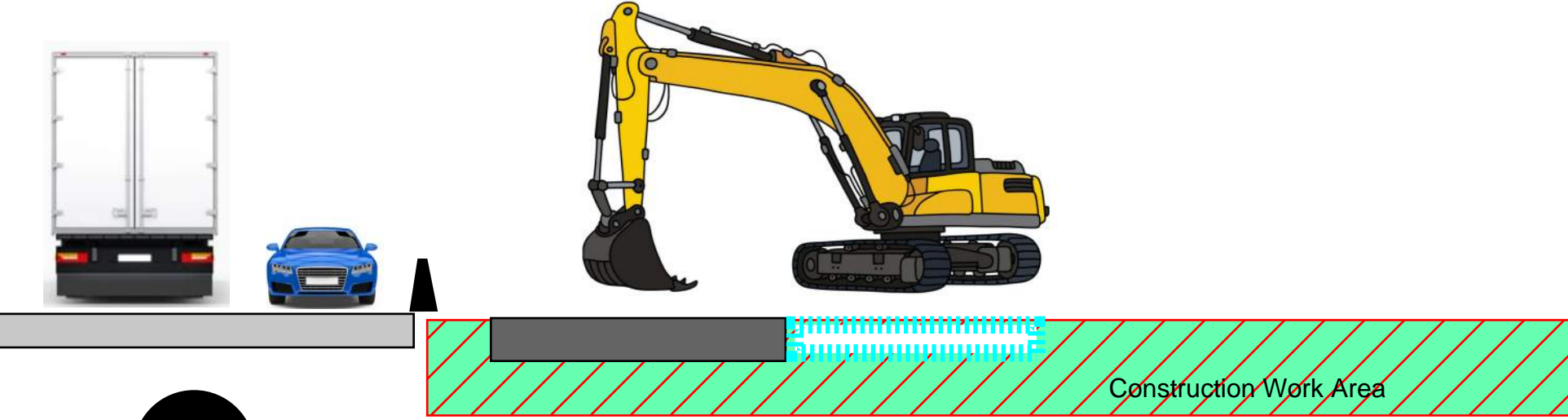
Roads open & in public use

Construction Work Area Phase 1

Construction Work Area Phase 2

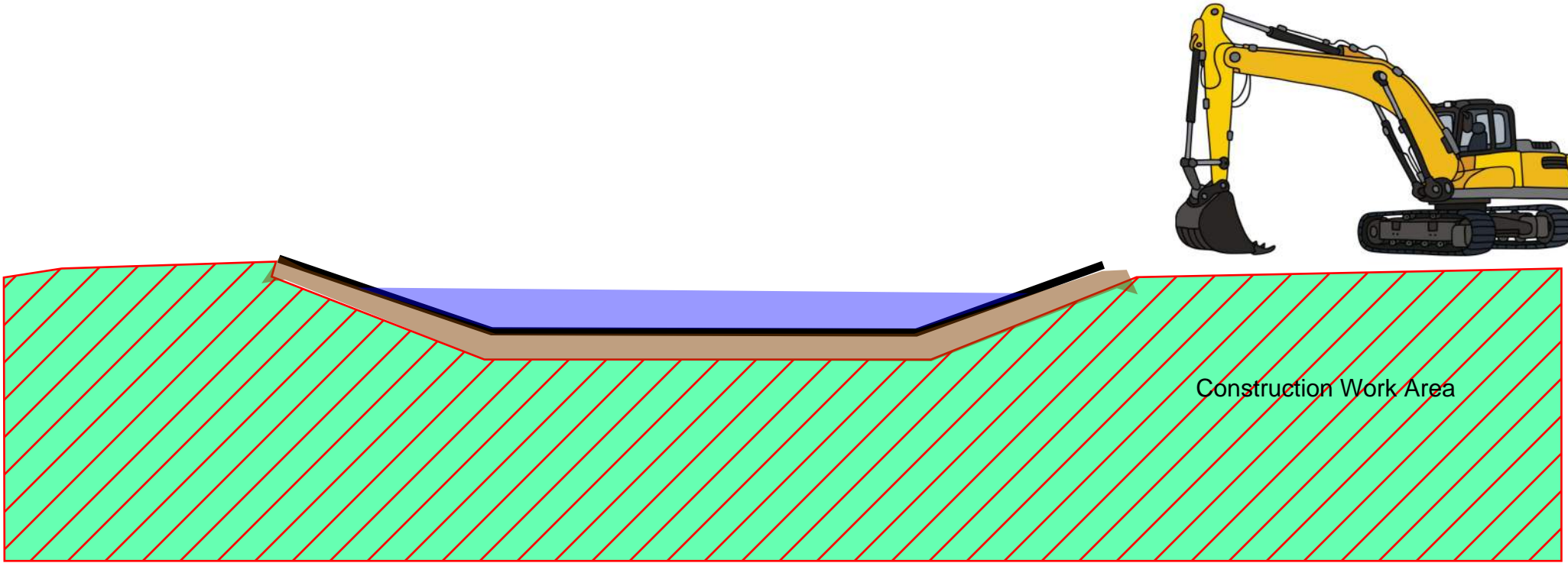
**Construction works - New carriageway**

With traffic diverted on the westbound carriageway, works can take place on modifying and widening the eastbound carriageway to suit the new road alignment. This will involve changes to the existing carriageway and widening works. Excavated material will be transported to local stockpile areas for reuse, with imported aggregate and asphalt material brought in for the works. Kerbs and drainage will be installed, followed by the new road being constructed with the aggregate and asphalt.



**Construction works - Attenuation pond**

To help manage groundwater on the scheme and mitigate the impacts of surface run-off overloading the existing drainage network, attenuation ponds are required. They will be construed with minimal impacts on the live A66 and will further help biodiversity within the local area. Plant machinery will be used to excavate and line the attenuation pond.



**Important** - Some features shown on this drawing may be different from those shown in the Maps Books. The purpose of the drawings highlight elements of the construction works and where discrepancies with the Map Books exist, the Map Book versions should be regarded as showing the preliminary design.

**RESIDUAL DESIGN HAZARDS**  
(The following information has been collected from Preconstruction information and the Amey Arup DJV CDM Hazard Management Process.)

- NOTES**
- All levels are in metres above Ordnance Datum.
  - All dimensions are in metres unless otherwise stated.
  - This drawing is to be read in conjunction with all other relevant drawings and the accompanying technical note HE565627-AMY-HGN-S08-TN-CH-000001.
  - The drainage design is of a sufficient level of detail to advise land take requirements and early stage design feasibility. Further design development to be undertaken.
  - The culvert naming convention is scheme specific for design coordination purposes only. A more detailed naming convention is to be agreed at a later design stage.
  - The vehicle restraint system layout is a high level assessment and requires a detailed risk assessment (RRRAP) of the hazards present in the design.
  - Road markings, lighting columns and structures are indicative only and require further design development.
  - Traffic sign faces and locations are indicative only. Traffic signs will be developed at detailed design.
  - Boundary treatments, such as fencing, are subject to agreement with landowners and detailed design.
  - Proposed walking, cycling and horse riding routes are indicative only and subject to agreement with local authority and landowners.
  - A high level assessment of technology requirements has been completed, with allowance made in the red line boundary for any new installations.

- KEY**
- Green line boundary
  - Existing layout
  - Existing watercourse
  - Proposed layout
  - Highway structure
  - Culvert
  - Cut-off drain
  - Filter Drain
  - Proposed boundary treatment
  - Proposed gate
  - Traffic sign
  - Proposed lighting column
  - Proposed vehicle restraint system
  - Headwall
  - Pipe to Outfall
  - Earthworks
  - Pond
  - Proposed Bridleway
  - Abandoned Bridleway
  - Existing Bridleway
  - Proposed Footway/Footpath
  - Abandoned Footway/Footpath
  - Existing Footway/Footpath
  - Proposed Shared Cycle Footway
  - Abandoned Shared Cycle Footway
  - Existing Shared Cycle Footway
  - Farm Track/Access
  - Compound/Storage Area
  - Demolished buildings
  - AONB
  - Potential Environmental Mitigation

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ISSUED FOR MODEL FREEZE D					
P01	SHOT	SHOT	THAL	RPEA	---
	18/02/21	18/02/21	18/02/21	18/02/21	---
UPDATED FOR MODEL FREEZE E					
P02.1	---	---	---	---	---
	---	---	---	---	---
Revision details					
Revision	Created	Checked	Reviewed	Approved	Authorised
	dd/mm/yy	dd/mm/yy	dd/mm/yy	dd/mm/yy	dd/mm/yy

Designer  
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**A66 NTP** Integrated Project Team

Client  
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M1 3BN

**highways**  
england

Project Name  
**A66 Northern Trans-Pennine**

Drawing Title  
**Scheme 8: Cross Lanes to Rokeby  
Black Option  
General Arrangement  
Sheet 3 of 5**

Project Ref. No. Stage Scale : 1:1000 @ A0  
--- PCF3 Dimensions : M

Drawing Number  
Project | Originator | Volume |  
**HE565627 - AMY - HGN -**  
**S08 -DR-CH- 600003**  
Location | Type | Role | Number

Suitability Suitability Description Revision  
S0 Initial non-contractual code P02.1



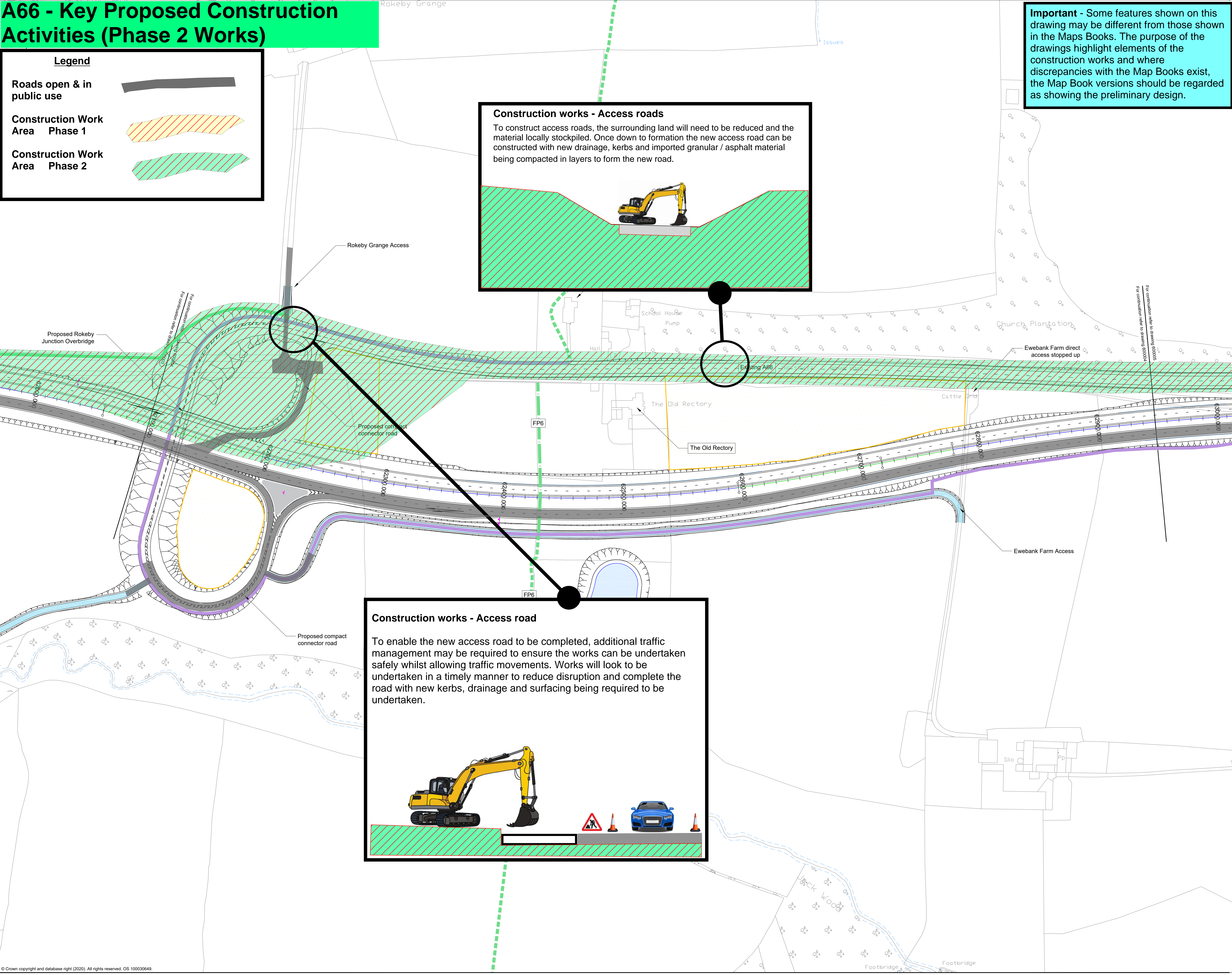
# A66 - Key Proposed Construction Activities (Phase 2 Works)

**Legend**

Roads open & in public use

Construction Work Area Phase 1

Construction Work Area Phase 2



**Important** - Some features shown on this drawing may be different from those shown in the Maps Books. The purpose of the drawings highlight elements of the construction works and where discrepancies with the Map Books exist, the Map Book versions should be regarded as showing the preliminary design.

**RESIDUAL DESIGN HAZARDS**

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- NOTES**
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**KEY**

Green line boundary

Existing layout

Existing watercourse

Proposed layout

Highway structure

Culvert

Cut-off drain

Filter Drain

Proposed boundary treatment

Proposed gate

Traffic sign

Proposed lighting column

Proposed vehicle restraint system

Headwall

Pipe to Outfall

Earthworks

Pond

Proposed Bridleway

Abandoned Bridleway

Existing Bridleway

Proposed Footway/Footpath

Abandoned Footway/Footpath

Existing Footway/Footpath

Proposed Shared Cycle Footway

Abandoned Shared Cycle Footway

Existing Shared Cycle Footway

Farm Track/Access

Compound/Storage Area

Demolished buildings

AONB

Potential Environmental Mitigation

	---	---	---	---	---
	---	---	---	---	---
P01	ISSUED FOR MODEL FREEZE D				
	SHOT	SHOT	THAL	RPEA	---
	19/02/21	19/02/21	19/02/21	19/02/21	---
P02.1	UPDATED FOR MODEL FREEZE E				
	---	---	---	---	---
	---	---	---	---	---
Revision	Revision details				
	Created	Checked	Reviewed	Approved	Authorised
	ddmm/yy	ddmm/yy	ddmm/yy	ddmm/yy	ddmm/yy

Designer

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10 Funnal Street  
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**Integrated Project Team**

Client

3 Piccadilly Place  
Manchester  
M1 3BN

Project Name

A66 Northern Trans-Pennine

Drawing Title

Scheme 8: Cross Lanes to Rokeby  
Black Option  
General Arrangement  
Sheet 4 of 5

Project Ref. No.	Stage	Scale : 1:1000	@ A0
		Dimensions : M	
---	PCF3		

Drawing Number

Project | Originator | Volume |

HE565627 - AMY - HGN -

S08 -DR-CH- 600004

Location | Type | Role | Number

Suitability	Suitability Description	Revision
S0	Initial non-contractual code	P02.1



A66 - Key Proposed Construction Activities (Phase 2 Works)

Legend

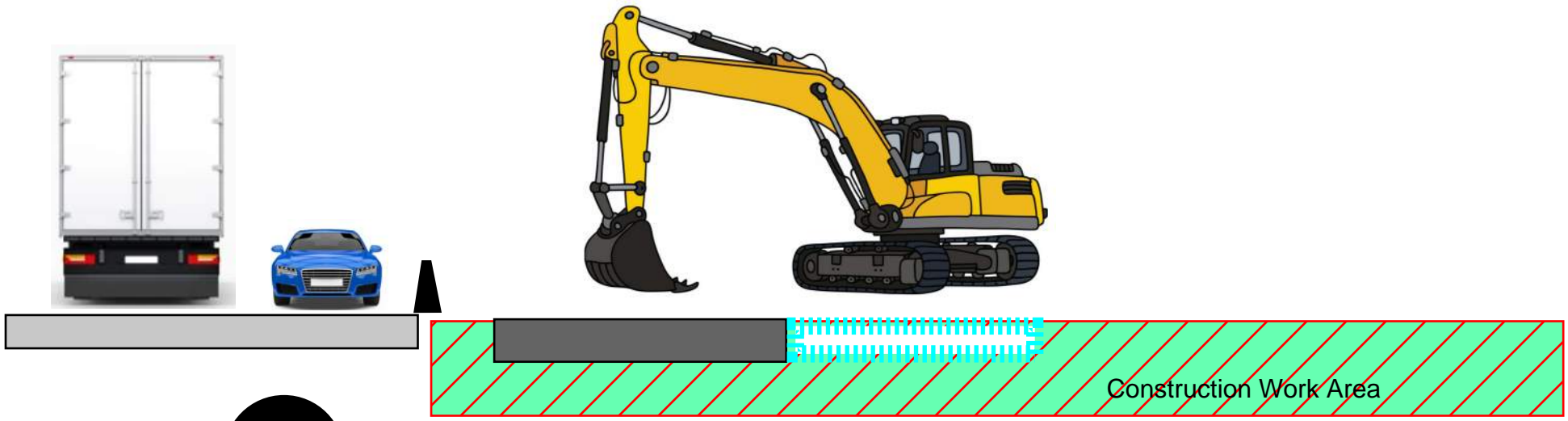
Roads open & in public use

Construction Work Area Phase 1

Construction Work Area Phase 2

Construction works - New carriageway

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RESIDUAL DESIGN HAZARDS

(The following information has been collected from Preconstruction Information and the Amey Arup DJV CDM Hazard Management Process.)

NOTES

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KEY

Green line boundary

Existing layout

Existing watercourse

Proposed layout

Highway structure

Culvert

Cut-off drain

Filter Drain

Proposed boundary treatment

Proposed gate

Traffic sign

Proposed lighting column

Proposed vehicle restraint system

Headwall

Pipe to Outfall

Earthworks

Pond

Proposed Bridleway

Abandoned Bridleway

Existing Bridleway

Proposed Footway/Footpath

Abandoned Footway/Footpath

Existing Footway/Footpath

Proposed Shared Cycle Footway

Abandoned Shared Cycle Footway

Existing Shared Cycle Footway

Farm Track/Access

Compound/Storage Area

Demolished buildings

AONB

Potential Environmental Mitigation

---				
ISSUED FOR MODEL FREEZE D				
SHOT	SHOT	THAL	RPEA	---
19/02/21	19/02/21	19/02/21	19/02/21	---
UPDATED FOR MODEL FREEZE E				
---	---	---	---	---
P02.1				
---				
Revision details				
Revision	Created	Checked	Reviewed	Approved
---	ddmm/yy	ddmm/yy	ddmm/yy	ddmm/yy

Designer

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Integrated Project Team

Client

3 Piccadilly Place  
Manchester  
M1 3BN

highways england

Project Name

A66 Northern Trans-Pennine

Drawing Title

Scheme 8: Cross Lanes to Rokeby  
Black Option  
General Arrangement  
Sheet 5 of 5

Project Ref. No.

---

Stage

PCF3

Scale

1:1000

@ A0

Dimensions

M

Drawing Number

Project

HE565627 - AMY - HGN - S08

Originator

Volume

1

Number

1

Location

DR-CH-600005

Type

Role

Number

1

Suitability

S0

Suitability Description

Initial non-contractual code

Revision

P02.1